

Development Services Department
Engineering Division
125 W Mountain St
Fayetteville, AR 72701
479-575-8206



Driveway, Sidewalk and Curb Cut Application

- Driveway and Curb Cut: \$25.00 fee.
- Sidewalk: \$25.00 fee.
- Driveway, curb cut and sidewalk: \$25.00 fee

Date: _____

Site Location and/or Parcel #: _____

Subdivision: _____

Lot: _____ Block: _____

- Sidewalk Along: _____ side-N S E W
Length _____ Width _____
- Curb cut Along: _____
Length _____ Width _____
- Driveway approach
- Access Ramp
- Other ADA requirements
- Other item Describe: _____

Sketch of Proposed Work
(If plans not provided)

All work to be done in compliance with City Ordinances. Grades will be established by City at no cost to owner. Applicant shall notify Sidewalk/ADA Administrator prior to performing work requiring inspection.

Owner:

Name: _____

Address: _____

Phone: _____

Contractor:

Name: _____

Address: _____

Phone: _____

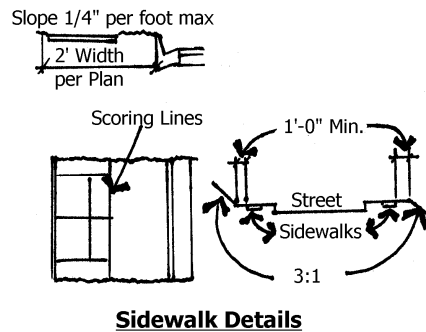
This permit expires three (3) months after date of issued and may be renewed upon application. Ref: 171.13 & 166.08

171.13 Sidewalk And Driveway Specifications

(A) Sidewalks.

- (1) Grades; establishment of property lines. All sidewalks, streets curbing and guttering, and driveway approaches shall be constructed in grades as established by the city official. It shall be the responsibility of the owner to establish property lines by competent survey at his/her own expense.
- (2) Minimum width of sidewalks. The minimum width of sidewalks shall follow the guidelines of the Master Street Plan, of the General Plan.
- (3) Sidewalk distances from the curb. The sidewalk setback distance from the curb shall follow the guidelines of the Master Street Plan, of the General Plan.
- (4) Minimum distance between sidewalk and slope. There shall be a minimum of one (1) foot between the sidewalk and the beginning of a slope as shown on the typical section detail.
- (5) Cement-concrete requirements. All side-walks, street curbing and guttering, and concrete driveway approaches shall be constructed of a Portland cement concrete mixture which will produce a concrete of a compressive strength of 3,000 pounds per square inch after 28 days set under standard laboratory methods.
- (6) Minimum thickness of sidewalks and driveway approaches.
 - (a) Sidewalks. The minimum thickness of sidewalks shall be four (4) inches with any fill materials required for residential or commercial sidewalks consisting of approved compacted base material.
 - (b) Residential driveway approached. The minimum thickness of residential driveway approaches shall be four (4) inches with four inches of compacted base material or six-inch by six-inch 10-gauge reinforcing steel.
 - (c) Commercial driveway approach. The minimum thickness of commercial driveway approaches shall be six (6) inches with six (6) inches of compacted base matter or six-inch by six-inch 10-gauge reinforcing steel.
- (7) ADA guidelines. Sidewalks shall conform to ADA guidelines.
- (8) Continuous through driveway. Sidewalks shall be continuous through driveways with a cold joint or expansion joint at the edge of the sidewalk opposite the street.

- (9) Edge adjacent to street. The sidewalk edge adjacent to the street shall have at least one (1) inch deep grooved joint mark (can be a cold joint) to clearly define the sidewalk through the driveway and approach.
- (10) Sidewalk elevation. The sidewalk elevation shall be two (2) percent above the top of the curb, sloping two percent towards the curb (one-fourth inch in each foot). This elevation shall be continuous through the driveway approach.
- (11) Slope. Sidewalks that are to be constructed adjacent to the curb shall be so located at their intersection with the approach and the driveway that the ADA requirement of the two (2) percent maximum vertical slope is met.
- (12) Driveway approach. The area remaining between the sidewalk and the flow-line of the gutter, called the approach to the driveway, shall slope up to the elevation of the sidewalk.
- (13) Joint material. Wood shall not be acceptable in sidewalks for expansion joints. The joint material shall be the same as approved for AHTD sidewalk construction (AASHTO M 213).
- (14) Expansion joint. Full depth expansion joints (four inches) shall be provided at intervals not greater than 50 feet. One-quarter depth (one inch) weakened plane joints, or saw-cut joints, shall be placed in sidewalk at regular intervals not greater than 10 feet apart.
- (15) Inspection. All sidewalks and curb cuts made for driveway approaches require an inspection prior to the concrete pour.
- (16) Materials. The material used for sidewalk construction shall be Portland cement concrete having a broom finish. The use of other materials must have the approval of the sidewalk and trails coordinator.
- (17) Edges. All sidewalks shall have one-half (1/2) inch rolled edges.
- (18) Removal/replacement. Removal and replacement of broken sidewalks require vertical saw-cuts on both ends of the sidewalk being replaced.
- (19) Curing compound. All sidewalks and driveway approaches require the application of a concrete curing compound or the concrete is to be kept moist for seven (7) days.



(B) Driveway approaches. (See: Illustration, Standard Driveway Approach-Residential)

(1) Curb removal. Curb, if existing, shall be removed for full width of the drive approach.

(2) Concrete removal. All concrete to be removed shall be saw-cut.

(3) Horizontal curb cut. Horizontal curb cutting along the flow-line of the gutter is allowed.

(4) No horizontal curb cut. If no horizontal curb cut is made, complete curb and gutter removal is required.

(5) Broken edges of saw-cuts. Broken edges of saw-cuts caused by demolition require a new saw-cut.

(6) Width of driveway approaches.

(a) Commercial.

(i) The width of commercial driveway approaches shall not exceed 39 feet measured at right angles to the centerline of the driveway approach for driveways that are one-way in and two-way out.

(ii) The width of commercial driveway approaches shall not exceed 27 feet measured at right angles to the centerline of the driveway approach for driveways that are one-way in, one-way out.

(iii) The width of commercial driveway approaches shall not exceed 15 feet measured at right angles to the centerline of the driveway approach for driveways that are one-way in or out.

(iv) The minimum width of commercial driveway approaches shall be 12 feet for one-way in or one-way out and 24 feet for one-way in and one-way out.

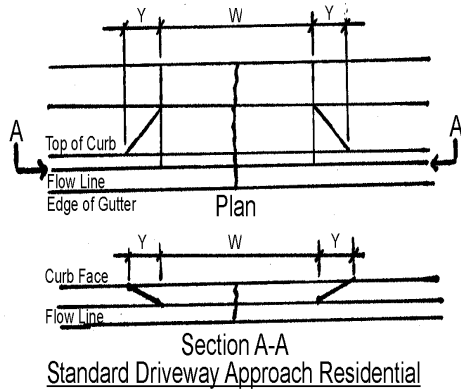
(b) Residential. The width of residential driveway approaches shall not exceed 24 feet measured at right angles to the centerline if the driveway approach and the minimum width shall not be less than 10 feet.

(7) Safety zones. Safety zones between driveway approaches shall not be less than the distance designated for distances between curb cuts in the standards for street design. On streets of higher use designation than collector, the standards for collector streets shall apply. Curbs shall be installed to prohibit vehicle parking and access in safety zones. The barrier line nearest the street or highway shall be on line with existing curbs, or established curb lines, but not less than 22 feet from the center of the pavement; provided the city official does not require a greater distance when needed to preserve the safety and utility of the street or highway, or provide conformance with proposed street or highway improvements. The curb cut for a driveway approach (other than for a single-family residential lot) shall be located a minimum of 122 feet from the nearest property line; curb cuts between driveway approaches on single-family residential lots shall be separated by a minimum of 10 feet. The curb cut for driveway approach on a single-family residential lot shall be located a minimum of 5 feet from the side property line; provided, this requirement shall not apply to a joint driveway (driveway approach providing access to two adjoining lots), or residential lots on the turning circle of a cul-de-sac.

(8) Safety zones at intersections and corners. No driveway approach shall be constructed less than the distance from the corner of a street or highway intersection designated in the standards for street design, provided the city official may require a greater distance between the corner and the driveway approach to preserve the safety of the street or highway; provided further the city may require the property owner to designate a driveway approach "for entrance only" or "for exit only" where more than one driveway approach provides access to the property in order to preserve the safety of the street or highway. On streets of a higher use designation than collector, the standards for distances between curb cuts for collector streets shall apply.

(9) Variations. The city official may grant variations where strict enforcement would be impractical due to circumstances unique to the individual property under consideration.

Residential Standard Driveway Approach



- (C) Access ramps. (See: Illustration guidelines: Fig. 11, and Fig. 12)
- (1) ADA requirements. Access ramps shall conform to ADA requirements.
 - (2) Sidewalk intersections. Access ramps shall be installed at all sidewalk intersections with the street.
 - (3) Parallel direction. Access ramps shall be installed in a direct parallel direction to the sidewalk.
- *Note--Therefore, any curb radii will require two access ramps.
- (4) Raised access ramp. The raised or built-up access ramp shall not be installed on public streets without the approval of the street superintendent.
 - (5) Minimum width. The minimum width of a curb ramp shall be 36 inches and 48 inches is recommended, exclusive of flared sides.
 - (6) Slope. The slope of the ramp shall not be greater than 8.33% (1:12).
 - (7) Slope of flared sides. The slope of the flared sides shall not be greater than 10% (1:10).
 - (8) Broom finish. Access ramps shall have a broom finish.

Measurement of Curb Ramp Slopes

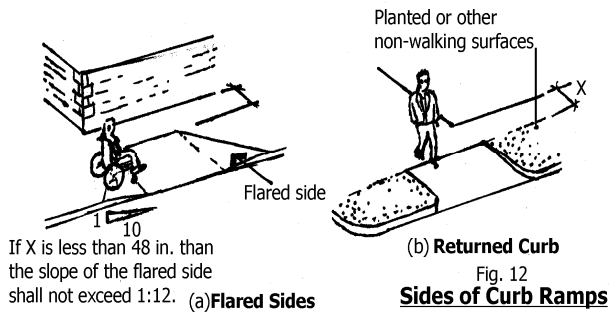
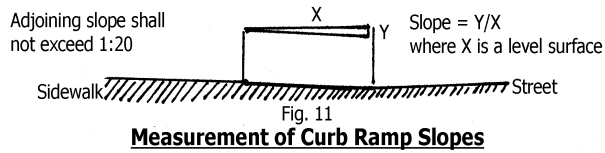
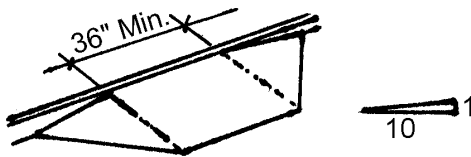


Fig. 11

Built-up Curb Ramp



Built-Up Curb Ramp

Fig. 12

(Code 1965, §18-28; Ord. No. 1790, 3-15-71; Ord. No. 2629, 5-6-80; Ord. No. 3117, 9-3-85; Code 1991, §98.67; Ord. No. 4005, §1, 11-19-96; Ord. No. 4100, §2 (Ex. A), 6-16-98)

- (F) Access Management. Safe and adequate vehicular, bicycle, and pedestrian access shall be provided to all parcels. Local streets and driveways shall not detract from the safety and efficiency of bordering arterial routes. Property that fronts onto more than one public street shall place a higher priority on accessing the street with the lowest functional classification, ex. Local and Collector. In a case where the streets have the same classification, access shall be from the

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lower volume street, or as determined by the City Engineer.

(1) Curb cut *separation*. For purposes of determining curb cut or street access separation, the separation distance shall be measured along the curb line from the edge of curb cut to the edge of curb cut/intersection. The measurement begins at the point where the curb cut and intersecting street create a right angle, i.e., the intersection of lines drawn from the face-of-curb to face-of-curb. The measurement ends at the point along the street where the closest curb cut or street intersection occurs; again, measured to the point where the curb cut or intersecting streets create a right angle at the intersection of face-of-curb. In all cases curb cuts shall be a minimum of five (5') feet from the adjoining property line, unless shared.

(2) Separation for two-family, three-family, multi-family and nonresidential development.

(a) Principal and Minor Arterial Streets. Where a street with a lower functional classification exists that can be accessed, curb cuts shall access onto those streets. When necessary, curb cuts along arterial streets shall be shared between two or more lots. Where a curb cut must access the arterial street, it shall be located a minimum of 250 feet from an intersection or driveway.

(b) Collector Streets. Curb cuts shall be located a minimum of 100 feet from an intersection or driveway. When necessary, curb cuts along collector streets shall be shared between two or more lots.

(c) Local and Residential Streets. Curb cuts shall be located a minimum of 50 feet from an intersection or driveway. In no case shall a curb cut be located within the radius return of an adjacent curb cut or intersection.

(3) Separation for single-family homes.

(a) For all street classifications, curb cuts shall be located a minimum of 10 feet from another driveway. Driveways serving corner lots shall be located as far from the street intersection as possible while still meeting a 5 foot separation from an adjoining property line. In no case shall a curb cut be

located within the radius of an adjacent curb cut or street intersection.

(b) Arterial and Collector Streets. Individual curb cuts for along arterial and collector streets shall be discouraged. When necessary, curb cuts along arterial and collector streets shall be shared between two or more lots.

(3) Variance. In order to protect the ingress and egress access rights to a street of an abutting property owner, a variance to the curb cut minimums shall be granted by the Planning Commission to allow an ingress/egress curb cut at the safest functional location along the property. Such a curb cut may be required to be shared with an adjoining parcel if feasible. If a parcel on the corner of an arterial or collector street provides such short frontage along a major street that there is no safe ingress/egress functional location on that street, the Planning Commission may deny the curb cut or may limit such curb cut to ingress or egress only.

(5) Speed. All streets should be designed to discourage excessive speeds.

(G) Non-conforming Access Features.

(1) Existing. Permitted access connections in place on the date of the adoption of this ordinance that do not conform with the standards herein shall be designated as nonconforming features and shall be brought into compliance with the applicable standards under the following conditions:

(a) When new access connection permits are requested;

(b) Upon expansion or improvements greater than 50% of the assessed property value or gross floor area or volume;

(c) As roadway improvements allow.

(H) Easements. Utility and drainage easements shall be located along lot lines and/or street right-of-way where necessary to provide for utility lines and drainage. The Planning Commission may require larger easements for major utility lines, unusual terrain or drainage problems.

(Code 1965, App. C., Art. IV, §§C, D, F--H; Ord. No. 1750, 7-6-70; Ord. No. 1801, 6-21-71; Ord. No. 2196, 2-17-76; Ord. No. 2353, 7-5-77; Code 1991, §§159.45, 159.58, 159.51--159.53; Ord. No. 4100, §2 (Ex. A), 6-16-98; Ord. 4757, 9-6-05; Ord. 4919, 9-05-06; Ord. 5156, 8-5-08; Ord. 5296, 12-15-09; Ord. No. 5546, 12-04-12; Ord. No. 5642, 12-03-13)